



Funds Distribution Report

Recipient Organization:
Whidbey Island Maritime Heritage Foundation

Address:
24 Front St
Coupeville, WA 98239

Mailing Address:
PO Box 532
Coupeville, WA 98239

Contact:
<https://www.whidbeyislandmaritimeheritagefoundation.org>

Organization's General Goals:
Our mission is to create awareness of the maritime heritage of Whidbey Island, Coupeville, and Penn Cove by educating, promoting, and preserving its maritime traditions.

Date of Award: 2024 Q2 **Level:** \$500 to \$2,500

For more information, please read the attached report from Whidbey Island Maritime Heritage Foundation.

Disclaimer: This report may be a summary of content provided by the recipient, not always complete quoted material.



WHIDBEY ISLAND MARITIME HERITAGE FOUNDATION

P.O. Box 532
Coupeville, Washington 98239
www.whidbeyislandmaritimeheritagefoundation.org
www.schoonersuva.org

January 10, 2025

Tulalip Tribes Charitable Contributions
8802 27th Avenue NE
Tulalip, WA 98271-5050

Charitable Contributions Fund Report (Q2 2024 14.2)

Dear Marilyn Sheldon, Director,

We are pleased to report on the completion of our project which was made possible with the generous support of the Tulalip Tribes Charitable Contributions Fund. The funding paid for a vessel survey that will be used to prepare the historic Schooner Suva's nomination for the National Register of Historic Places (NRHP). The survey has been completed and Suva's listing on the NRHP's is anticipated this coming summer.

The vessel survey was conducted on October 22, 2014, by Aaron Bantstra of Marine Consultants, Bellingham, for a cost of [REDACTED]. The final Condition and Valuation Survey, which contains many photos, has since been issued and is attached. Also, in conjunction with the survey, six Oil Analysis Kits were purchased to evaluate the mechanical condition of the vessel's engine and transmission. The total price for the six kits was \$144.84, bringing the total vessel survey expenses to [REDACTED], which amount was covered by the Tulalip Tribes Charitable Fund donation of [REDACTED]. Attached are invoices for the costs involved.

The Whidbey Island Maritime Heritage Foundation is operated by a team of over 80 dedicated volunteers and serves some 1500 sailing adventure passengers each year in addition to providing approximately 2000 dockside tours. Your support on this important project for Schooner Suva is greatly appreciated by all involved.

Sincerely,

Gary Pursell
Project Coordinator, WIMHF

Marine Consultants

Marine Surveyors and Consultants

1901 Cornwall Ave PMB 815, Bellingham WA 98225
(360) 594-9926 aarondbandstra@gmail.com

WORK ORDER/INVOICE

File Number: S581024D

To: Coupeville Maritime Heritage Foundation, Gary Pursell

Thank you for your request to provide an in-water Condition and Valuation Marine Survey on the 58' Wooden Pilothouse Schooner SUVA in Oak Harbor, WA. I am scheduled to survey the vessel on October 22, 2024. You are welcome and encouraged to attend.

Summary of fees:

58' SV Condition and Valuation Survey: [REDACTED]

Travel to Oak Harbor: [REDACTED]

Total: (payment by check is preferred) [REDACTED]

You are also responsible for any yard fees.

Please review this Work Order/Invoice, sign it, and return it by email. The signed Work Order/Invoice and payment or arrangement for payment are required before the survey report can be issued.

After I have completed the physical observations of the vessel, I will, as an employee of my company, prepare a Condition and Valuation Marine Survey Report which you can use for insurance or financing purposes. I will be providing the survey in accordance with the Scope and Conditions listed below and on the attached Terms and Conditions page.

Scope and Conditions of the Survey:

During the process of conducting this survey, I will, as an employee of my company, Marine Consultants, Inc. observe the vessel and may provide opinions or make recommendations based on observations of the vessel. This work will include but is not limited to the following procedures:

- Documenting the hull identification number, official number, and engine number(s).
- Documenting the vessel's configuration, historic features and major equipment for its upcoming nomination to the National Register of Historic Places.
- Certain AC electrical tests.
- Observation of vessel systems in operation to determine apparent functional status where permission is granted and circumstances allow.
- Observation of spaces accessible without tools.
- Observation, acoustic sounding (hammer tapping), and possible spot moisture meter testing of wooden structures.
- Observation of the vessel with respect to USCG (United States Coast Guard) requirements, and ABYC (American Boat and Yacht Council), or NFPA (National Fire Protection Association) voluntary standards.

Note: The vessel may have been built before the enactment of some of the USCG requirements or ABYC and NFPA voluntary standards in effect today. The vessel is observed with reference to the current standards, and deficiencies thought to be important to the safety of the vessel and personnel are reported. This survey does not and cannot require complete compliance with all of the voluntary standards.

- Developing a list of findings/deficiencies observed, with recommendations for correction.
- Developing a list of advisory notes where deemed appropriate.
- Research after the physical survey to determine an opinion of estimated Fair Market Value and Replacement Value for the vessel.

- Preparation and delivery of a Marine Condition and Valuation Survey report, in PDF form, by electronic transmittal.

This vessel survey does not include an examination of the sails or the upper portions of the mast and rigging, although the portions of the rig and spars accessible from deck level are observed.

A rigger can be separately engaged to perform a complete rigging inspection, if desired.

If the value and condition of sails is of interest to you, their evaluation by a sailmaker should be separately arranged.

Routine sailboat maintenance should include annual inspection and service of the rig by a qualified rigger. Additionally, chainplates and their fasteners should be periodically examined by disassembly.

I will observe and report observations about the engine and will need to attend a trial run, but if a more detailed engine survey and oil analysis is desired, that should be separately arranged.

Unless separately arranged and agreed, this examination is conducted without making removals or opening up to expose areas or components ordinarily concealed, or test boring, testing for tightness, or pressure testing tanks, or making an exhaustive examination of the AC or DC electrical system or bonding system, and does not, therefore, address any damages and/or deficiencies which might have been revealed if such procedures had been executed. No evaluations are made and no opinions are offered relative to the vessel's engineering, performance, or stability, or of the sizing or suitability of any installed components, design, or feature.

The report is only a statement of opinion and is neither a guarantee nor a warranty relative to: the merchantability of the vessel, the valuation of the vessel, the condition of the vessel or its machinery or equipment, or any unforeseen or undetected damages or other conditions that may exist.

The limited report is issued in accordance with the **Terms and Conditions** attached below which **Terms and Conditions** apply to the /attending marine surveyor or Marine Consultants, Inc. Acceptance of the report or its use for any purpose serves as acknowledgement and agreement with these terms and conditions.

Please execute the signature line below to formalize our service agreement. A signed copy of this Work Order/Invoice and payment for the survey is required before issuance of the survey report.

Work Order/Invoice
File number: S581024D
Page 4 of 5

Should you have any comments or questions, please do not hesitate to contact me. I can best be reached at (360) 594-9926, or by email at aarondbandstra@gmail.com

Sincerely,



Marine Consultants
Aaron Bandstra NAMS-CMS



Gary Pursell
Gary Pursell (Oct 19, 2024 13:54 PDT)
Client Signature
10/19/2024
Date

Gary Pursell
Print Name

TERMS AND CONDITIONS

Marine Consultants 2023 LLC, doing business as Marine Consultants (hereafter referred to as MC) agrees to undertake the work requested by Customer only on the following terms and conditions which shall apply to all work done by MC and all reports relating to such work.

1. Additional Services: Any additional work requested or authorized by Customer, either verbally or in writing, shall be subject to these terms and conditions. Additional work performed will be charged at MC's normal hourly rate.

2. Expenses: Customer shall pay MC all costs, including but not limited to travel expenses, hotel, meals, lodging, telephone, and other expenses relating to the work requested.

3. Payment and Interest: Payment of all fees and expenses shall be due upon completion of the work unless other arrangements are made. All payments, if unpaid when due, shall bear interest at the rate of 1.5% per month from the date due until paid.

4. Lien: Customer grants MC a lien (including a maritime lien) on the vessel and its equipment involved in this work order until all fees and expenses have been fully paid. MC shall have and retain all other legal rights it may have, until the fees and expenses have been paid in full. Customer, including the vessel, and/or the person directly contracting MC for this work remain jointly and severally responsible for the charges until fully paid. The person signing this work order warrants that he has authority to bind all such parties to these fees and expenses.

5. Limited Report: Customer is cautioned that this is a limited report representing a limited inspection by visual means and soundings. Inspection of areas normally concealed, areas requiring disassembly of the vessel, scaling of masts, or the operation of equipment is specifically not included unless otherwise agreed upon in writing. Such reports constitute only statements of opinion and are not to be construed or considered as representations, warranties, or guarantees. MC disclaims any expertise regarding insurance. Any statements made by surveyor are not to be construed in any way as bearing upon the possible existence of insurance coverage. Except as provided herein, MC makes no warranties, express or implied, regarding the work performed, including, without limitation, any implied warranties of merchantability or fitness for a particular purpose. The representations and warranties made by MC in this agreement apply only to the work performed by MC and will be invalid if the Customer is in breach of these terms and conditions.

6. Limited Liability: MC shall not be liable to Customer for any claim, loss, cost, penalty, or damages of whatsoever kind or nature arising out of, in connection with, or incident to, the work requested, except that caused by the direct sole negligence of MC. Negligence shall not be legally presumed but must be affirmatively established. MC shall not be liable in any event for any loss, cost, penalty, or damages in excess of the total cost paid by Customer for the work. MC shall not be liable to Customer except on the limited basis identified above. MC shall specifically not be liable for incidental, special or consequential damages, nor loss of use, loss of profits/earnings, crew wages, shares, salvage, repair, tug expense, demurrage, loss of time, loss of freight, loss of charter and/or similar and/or substituted expenses.

In addition, MC shall not be liable to Customer on any legal basis other than negligence as stated above. Liability to Customer for breach of contract, breach of warranty of workmanlike service, strict and/or products liability, liability for breach of warranties of merchantability or fitness for a particular purpose or any other legal theory or basis for liability, and liability (directly or indirectly) to Customer's insurers, are specifically agreed by Customer and MC to be excluded. It is understood by Customer that MC's charges for services are based upon this limited liability. MC agrees to assume additional liabilities only if requested by Customer and a written agreement setting forth liabilities and additional charges are signed by both MC and Customer.

7. USCG: The USCG and other government agencies may require additional equipment and/or modifications to the vessel depending upon its use. MC shall not be liable for anticipation of these requirements.

8. Markings: MC assumes no legal or financial liability for any cosmetic work necessary to remove marks or blemishes caused by the inspection.

9. Notice, Claim, Time Limits or Suit: MC shall in no event be liable even on the limited basis identified above, unless notice of claim thereof is presented in writing to MC within ninety (90) days of completion of the work. Customer must also commence suit on any claim or controversy arising under this work order or the work performed pursuant to the work order, within six (6) months from completion of work. If Customer fails to do either then MC is discharged from all liability to Customer on any basis.

10. Law and Venue: Any work performed by MC and any report issued by MC shall be construed in accordance with the laws of the State of Washington. Any action, claim, or suit between the parties must be brought in the state courts located at Bellingham, Whatcom County, Washington. The prevailing party in any litigation shall be entitled to recover all costs including reasonable attorneys' fees.

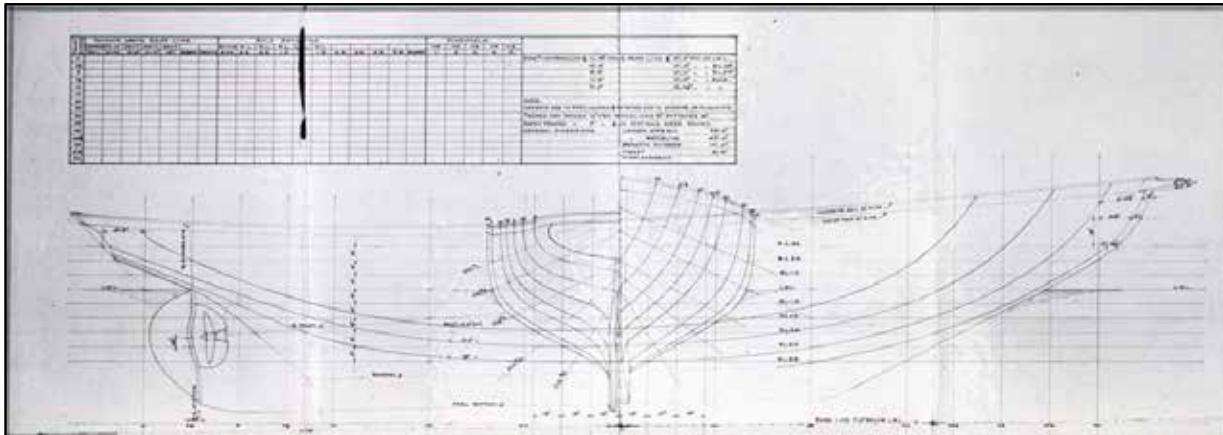
11. Specifications: MC shall not be liable for the accuracy of dimensions, capacities, ratings, equipment, inventory, etc. This data is often obtained from outside sources and is included for general descriptive purposes only.

12. General: These terms (together with any scope of work): (i) represents the entire understanding of the parties with respect to the subject matter covered; (ii) supersedes all prior and contemporaneous oral understandings with respect to such subject matter; (iii) may only be amended in a writing signed by both parties; (iv) binds and inures to the benefit of the parties and their respective successors, permitted assigns, agents and representatives; and (v) constitutes material consideration for the agreement between MC and Customer without which MC would not agree to perform the work. The failure of either party at any time to require performance of any provision of these terms shall not limit such party's right to enforce such provision, nor shall any waiver of any breach of any provision of these terms constitute a waiver of any succeeding breach of such provision or a waiver of such provision itself. Any provision of these terms which shall prove to be invalid, void, or illegal shall in no way affect, impair, or invalidate any other provision of these terms, and the remaining provisions shall nevertheless remain in full force and effect.

CONDITION AND VALUATION SURVEY

Vessel Name: SUVA
Vessel Type: Pilothouse Staysail Schooner
File No.: S581024D

Prepared for: Whidbey Island Maritime
Heritage Foundation
Date of Survey: October 22, 2024



Surveyed by:
Aaron Bandstra, NAMS-CMS, Marine Consultants
1901 Cornwall Ave PMB 815 Bellingham, WA 98225

This report is prepared subject to the terms and conditions on page 29.

GENERAL INFORMATION

Vessel Name	SUVA
Date of Survey	10/22/2024
Date of Report	10/28/2024
Home Port	Coupeville, WA
Location of Survey	City of Oak Harbor Marina, Oak Harbor, WA
Hull No.	NA
Official No.	225008
Hauled	No
Trial Run	No, inspections and systems run dockside
Afloat	Yes
Type of Vessel	Pilothouse Staysail Schooner
Designer	L.E. "Ted" Geary
Builder	Quan Lee, Hong Kong
Model year	1925
LOA	68'
LOD	58'
Beam	14'
Draft	6'-6" ft.
Registered Gross Tonnage	27
Registered Net Tonnage	18
Weather conditions	Overcast with sun breaks, light wind
Owner	Whidbey Island Maritime Heritage Foundation (WIMHF)
Requested by	Gary Pursell, Whidbey Island Maritime Heritage Foundation
People Present	David Young, Kirby Stevens and Aaron Bandstra

Note: The specifications noted for this vessel are obtained from outside sources, and have not been verified.

VESSEL PURPOSE AND HISTORY

The historic schooner SUVA is important to the identity of Coupeville, WA. In 1924 Coupeville resident Frank Pratt Jr. commissioned the design from renowned Seattle based Naval Architect L.E. “Ted” Geary. The schooner was then built in 1925 at the Quan Lee shipyard in Hong Kong and shipped to British Columbia, Canada where the spars were stepped before sailing to Coupeville where it was sailed by the Pratt family and became a fixture in the community. The schooner did eventually leave Coupeville, but was returned in 2015 when it was sold to the Coupeville Maritime Heritage Foundation, which was later renamed the Whidbey Island Maritime Heritage Foundation (WIMHF).

Today the schooner is an inspected passenger vessel and is used primarily for day sails from Coupeville, exploring the waters of Penn Cove. The schooner introduces people to these waters, with its rich biodiversity, long stewardship by the Lower Skagit Tribes, and more recent history of exploration, commerce, development and aquaculture.

The schooner is operated by a team of dedicated volunteers and paid seasonal captains. This team brings a wealth of experience in local history, technical expertise, sailing and navigation, and a passion for connecting people to our waterways.



The Coupeville waterfront, with SUVA moored at the end of the wharf. Photo courtesy of WIMHF.

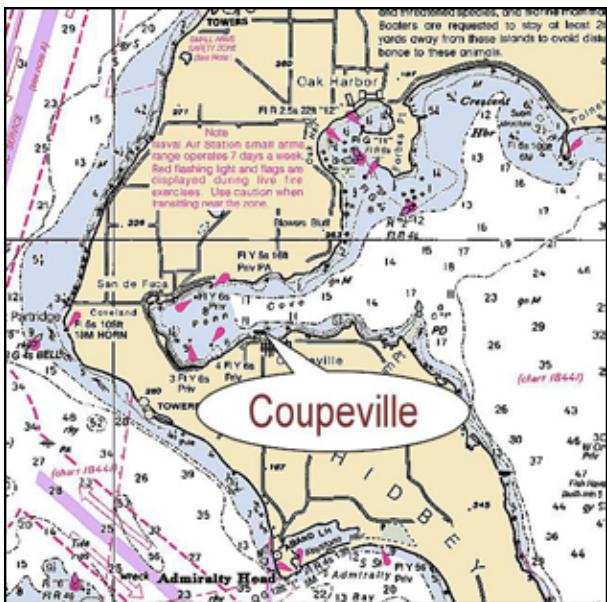


Photo courtesy of WIMHF

Historic Timeline:

1924: Frank Pratt Jr. of Coupeville WA commissions the design from L.E. "Ted" Geary N.A. of Seattle WA. Ted Geary was a prominent Naval Architect, with many of his boats well known and in use today, including

WESTWARD 1924 86' motor yacht

MALIBU 1926 100' motor yacht

BLUE PETER 1928 96' motor yacht

PIRATE 1926 R-class racing yacht

1925: SUVA was built in Hong Kong by shipbuilder Quan Lee, and shipped to British Columbia, Canada.

1940: SUVA was sold to Dietrich Schmidt.

1980: SUVA was sold to Bill Brandt of Olympia, WA.

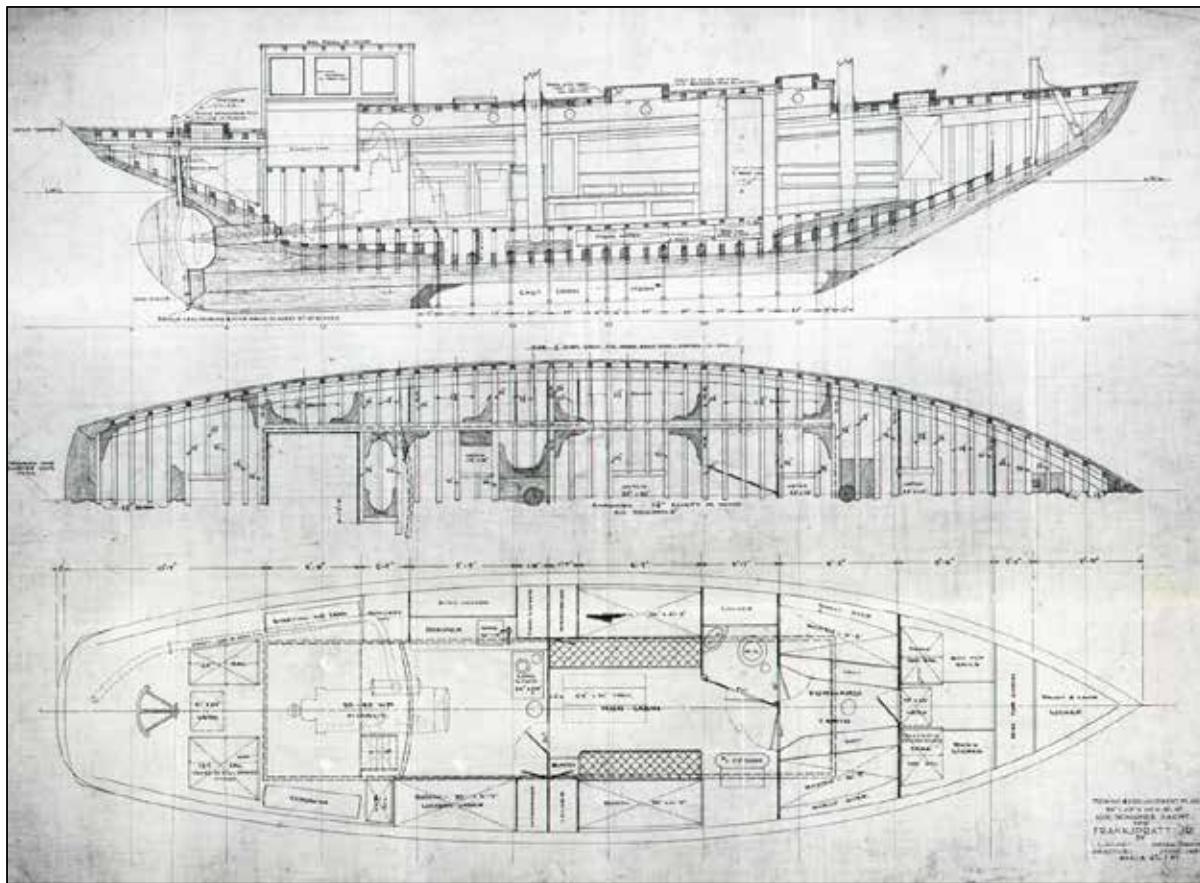
2009: SUVA was sold to Lloyd Baldwin of Port Townsend, WA.

2015: SUVA was sold to the Coupeville Maritime Heritage Foundation, which was later renamed the Whidbey Island Maritime Heritage Foundation (WIMHF).

2024: WIMHF prepares nomination for the National Register of Historic Places for SUVA with the National Park Service.

VESSEL DESCRIPTION

SUVA was designed with a curved bow, full keel with sloped forefoot, keel hung rudder and raked transom. The keel has external iron ballast bolted to the centerline keel timbers. These centerline keel timbers support floor timbers and the mast steps. The deck layout is that of a Pilothouse Staysail Schooner. Forward is a bowsprit with side planks, followed by a bronze twin gypsy Ideal windlass, and a hatch over the forward accommodations. Next aft is the foremast, followed by a long trunk cabin with portlights, hatches including a skylight butterfly hatch, mainmast and an aft pilothouse with large windows and doorway aft. Side decks lead around the trunk cabin and pilothouse to the aft deck, which has a hatch over the lazarette followed by the steering binnacle.

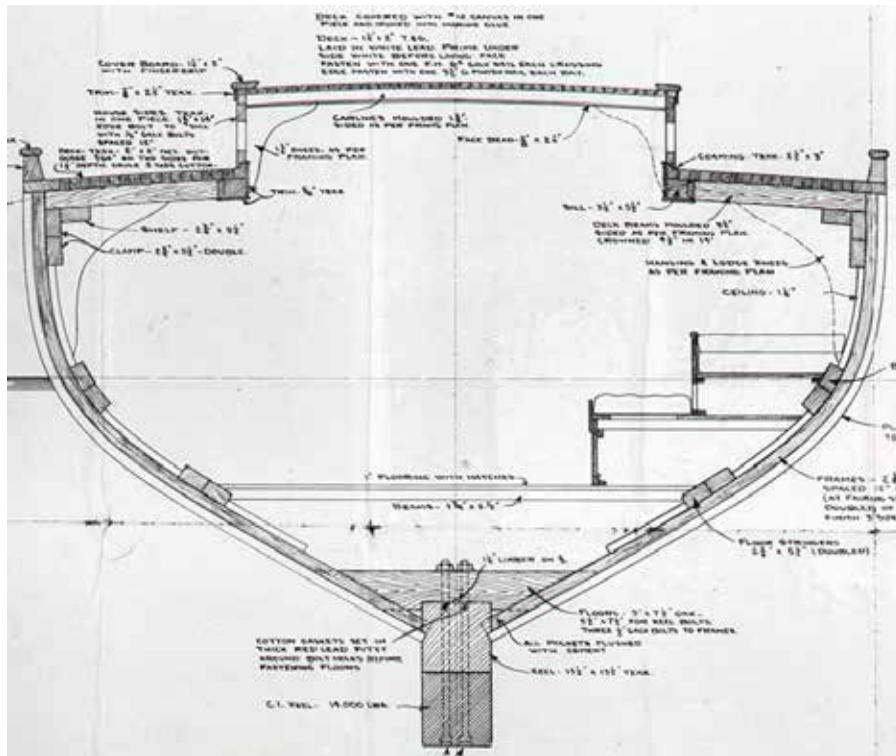


Interior accommodations: An anchor locker is in the forepeak, with aft access. A sail locker with outboard fuel tanks is next aft, followed by a stateroom with outboard berths with drawers below. The salon is next aft, with a port side head compartment and starboard side diesel heater, and port and starboard convertible settees with outboard berths and a central drop leaf table, and aft lockers. A doorway leads to the galley, which has a starboard side berth, port side galley counter and diesel stove, port aft engine access door, central aft engine box and starboard aft steps up to the pilothouse. The pilothouse has a port side helm and chart table, port aft raised settee and starboard aft door to the aft deck.

VESSEL CONSTRUCTION

SUVA is a traditionally built wooden vessel built to contemporary standards. During the course of the survey, some scantling dimensions were measured, and others are recorded from the original drawings.

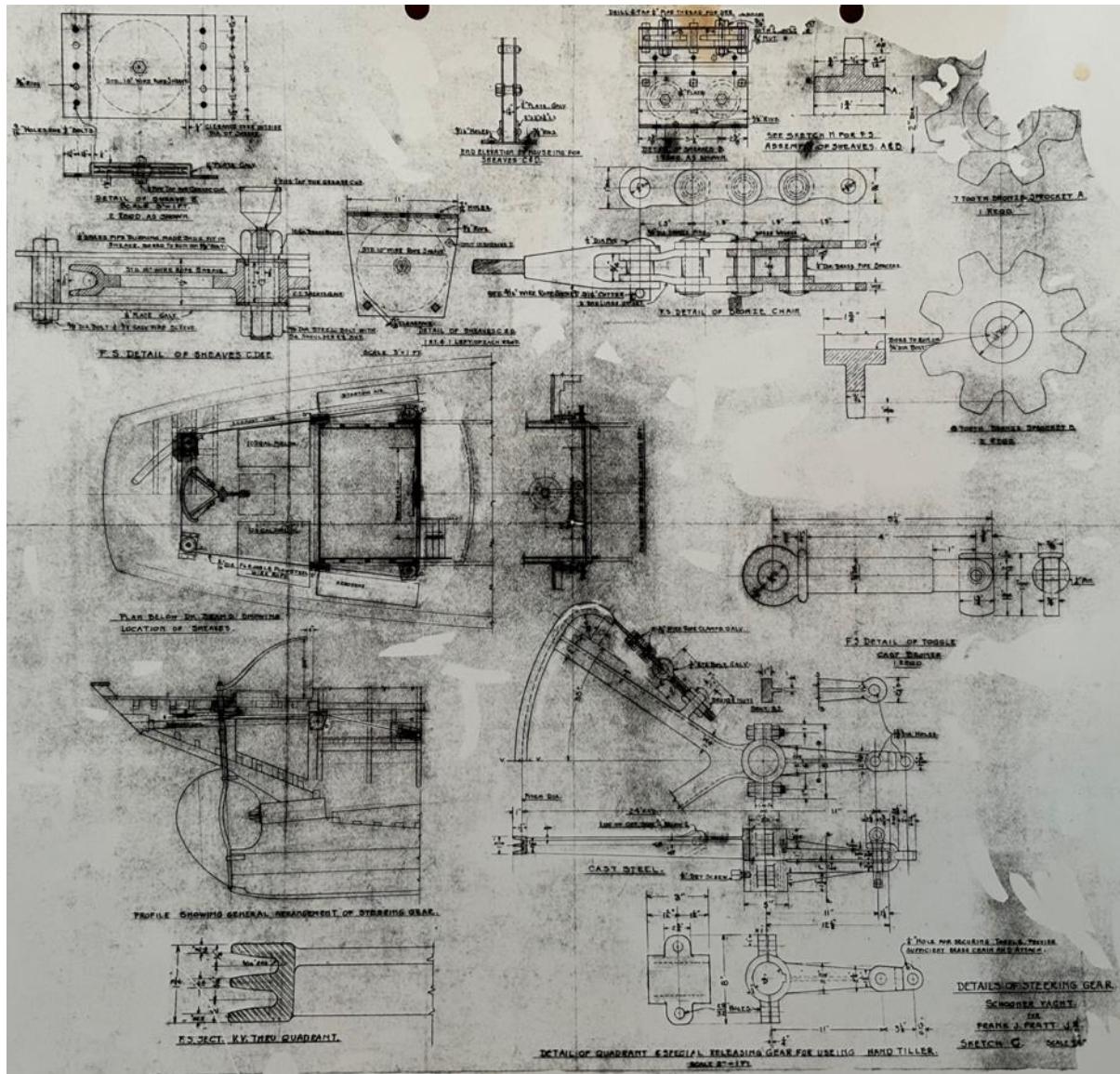
Keel timber	13-1/2" x 13-1/2" teak
Ballast	14,000 lbs. iron, with additional lead ingots
Keel bolts	1-1/4" steel rod threaded
Planking	1-7/8" teak, caulked
Original planking fasteners	5/16" x 4" and 1/4" x 3-1/2" iron spikes
New planking fasteners	#18 x 3-1/2" cut-thread hot-dipped galvanized screws
Frames	2-1/4" x 2-1/4" bent white oak on 12" centers
Interspersed Frames	3-1/4" x 2-1/2" sawn and doubled oak, every fifth frame
Floors	3" x 7-1/2" oak
Floor & Bilge Stringers	2-3/4 x 5-1/2" timber doubled
Shelf	2-3/4" x 5-1/2" timber
Clamp	2-3/4" x 5-1/2" timber doubled
Deck beams	2-3/4" x 3-1/2" timber
Decking	2-3/4" wide teak, caulked



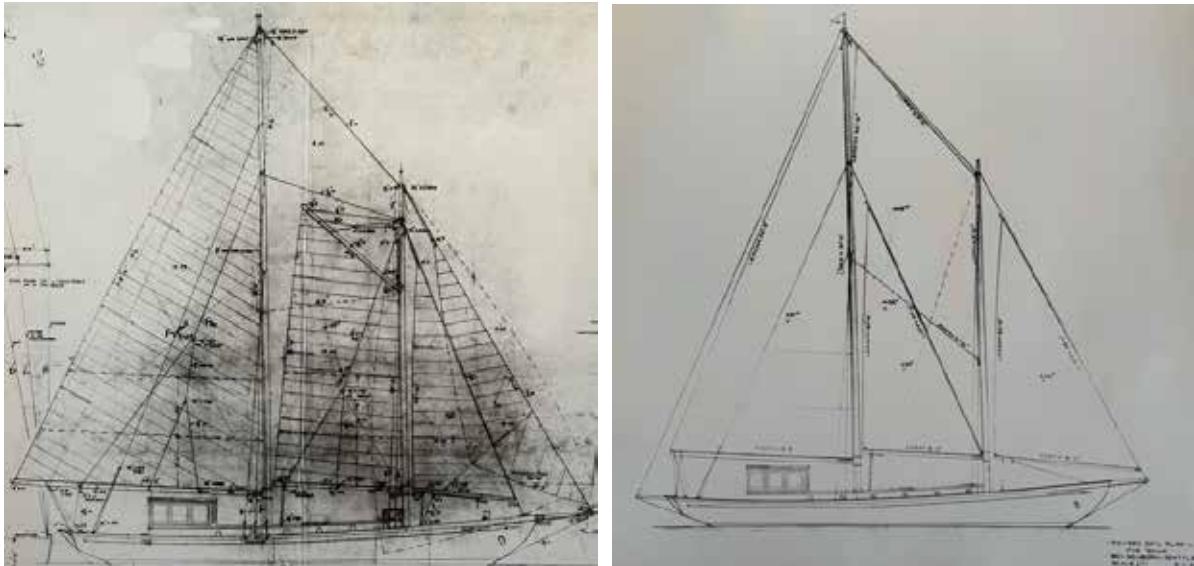
MAINTENANCE AND MODIFICATIONS

SUVA has been well maintained and modified during her nearly 100 year history. Major work is listed below:

Between 1925-1940: The vessel was originally built with tiller steering, and later wheel steering was added. See technical drawings below;



1940: The sail plan was redesigned by Ben Seaborn N.A. of Seattle. The original sail plan with a gaff foresail and marconi mainsail is shown on the left, and the updated sail plan is shown on the right.



1960: - The vessel was re-rigged as a Staysail Schooner, and the spars were replaced with Sitka Spruce.

- The original engine Lawson-Scott gasoline engine was replaced with a Detroit Diesel 453 diesel engine.

2007/2008: The rigging plan was revised by Antonio Salguero of the Port Townsend Shipwrights Co-Op. The foremast and bowsprit were lengthened, and the foremast stays replaced.



Photo courtesy of WIMHF

2014: Eight of fifteen keel bolts replaced. The bolts are reported to be custom machined and hot dipped galvanized.

2017: - Some standing rigging replaced with 1x19 stainless wire, and the bobstay as replaced with galvanized chain by Northwest Rigging of Anacortes, WA.

- The masts were reconditioned by Emerald Marine Carpentry (EMC) of Anacortes, WA.
- Removal of the starboard foremast chainplate, and replacement of sections of the clamp and framing by EMC.
- Replacement of some aft deck planks by EMC.
- Refastening of some stern planks by EMC.

2019: - Refastening of the garboards by EMC.

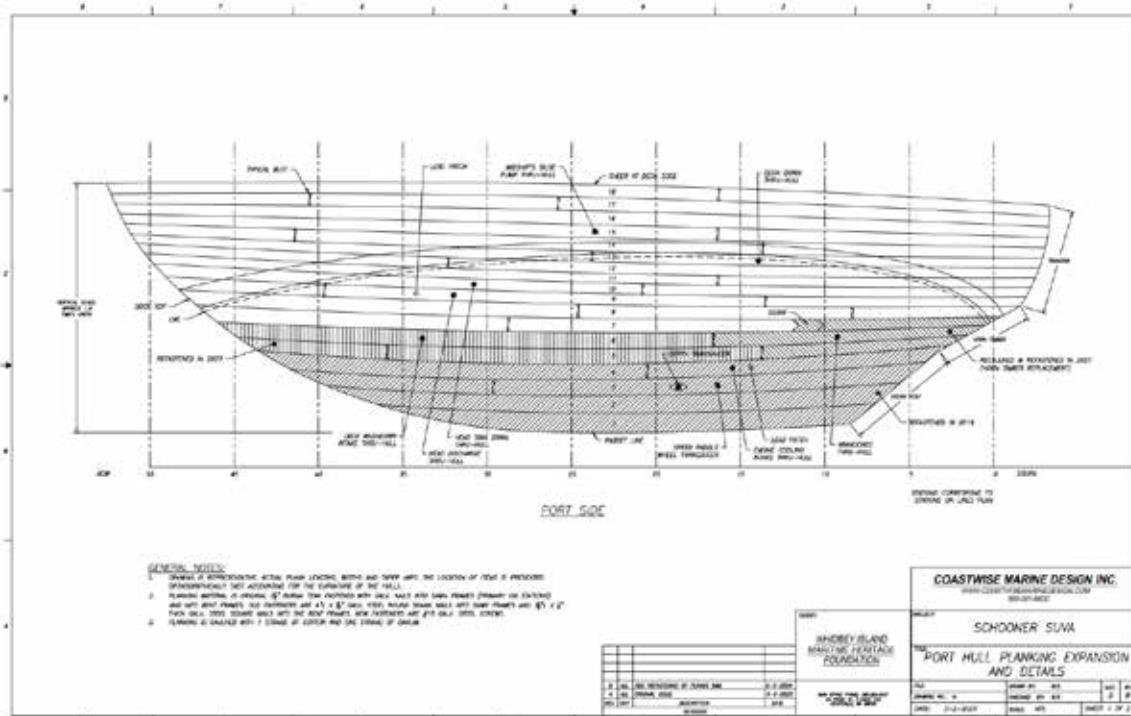
2021: - Topside and spar re-painting

- Replacement of the horn timber by EMC, see before & after photos below.



Photos courtesy of WIMHF

2023: - Documentation of hull planking, thru-hulls and re-fastening schedule by Coastwise Marine Design. The Port side plan is below, and the Starboard side is similar.



2024: - Completion of below-waterline planking re-fastening by EMC.



Photo courtesy of WIMHF



Examples of plank fasteners: Replacement #18 x 3 1/2" galvanized screw, and original iron spikes



Bottom paint renewed, photo courtesy of WIMHF

The future work to best maintain this historic schooner includes the following:

- Replace the 1960 Detroit Diesel engine
- Complete the keel bolt replacement
- Inspect the rig and replace any standing rigging as needed
- Coat and seal the teak decks to eliminate leaking
- Address the findings in this report

PROPELLION

Engine make and model	Detroit Diesel 5042-4000	The engine was run dockside. Oil leaks noted, but otherwise ran normally. Eventual replacement is planned.
Serial Number	4D-38979	
Rated power	107 HP @ 2400 RPM	
Indicated hours	4274	
Cooling	Fresh water with raw water heat exchanger	Functional
Exhaust	Wet exhaust with waterlift muffler	Elbow/muffler corroded and leaking
Controls	Mechanical cables	Functional

RUNNING GEAR

Shafts	2" stainless steel shaft	Not fully inspected
Struts and bearings	Cutless bearing in stern tube reported	Not inspected
Propeller	25" three blade Max Prop reported	Not inspected
Shaft seal	PSS Shaft Seal	Serviceable

STEERING & CONTROL

Type	Hydraulic steering aft station Cable steering pilothouse	Oil leak at helm pump (2) steering sheaves broken/seized
Rudder description	Timber keel hung rudder reported	Not inspected
Rudder bearings	Bronze	Not inspected

FUEL SYSTEM

Fuel Type	Diesel	Tank Material	Stainless steel
Fuel valves	Helm and foredeck	Tank quantity	(4) plus (1) decommissioned
Fuel tank(s) grounding	Yes	Tank Capacity	342 gallons total reported

The fuel tanks could not be fully inspected due to limited access, but where accessible appear to be in serviceable condition.

MARINE CONSULTANTS

Marine Surveyors and Consultants

BELLINGHAM, WA, USA

Fuel filters	Egnine: Dual Racor 75500 MAX Furnace: Racor spin-on	Furnace filter bowl not equipped with heat shield
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WATER SYSTEMS

Fresh water tanks	(2) 40 gallon stainless steel tanks	Serviceable
Water heater	AC/engine 11 gallon	Functional
Freshwater pumps	12VDC 2.8 GPM	Functional
Deckwash pump	Johnson WPS 5.2	Functional
Galley sink drain pump	Whale Gulper 220	Functional

THRU-HULL PLUMBING

Thru -Hull Assemblies	Engine intake	Bronze ¼ turn	Functional
	Eng rm. bilge discharge	Bronze ¼ turn	Functional
	Galley bilge discharge	Bronze ¼ turn	Functional
	Galley sink drain	Bronze ¼ turn	Functional
	St. Deck scupper drain	Tapered	Functional
	Pt. Deck scupper drain	Tapered	Inoperative
	Head/washdown intake	Bronze ¼ turn	Functional
	Head sink drain	Tapered	Functional
	Head discharge	Tapered	Functional
	Fwd bilge discharge	Bronze ¼ turn	Functional

BILGE PUMPS

Pump type	(3) Rule 2000 12VDC	Float switches and manaul switches, proven.
Engine driven bilge pump	Valving for two bilges	Pipe connection cracked

WASTE SYSTEMS

Head description	(1) electric head	Functional
System	Y valve to direct discharge overboard or to plastic holding tank, with level monitor	Plumbing is functional Holding tank level monitor inoperative

Holding tank discharge	Jabsco 50890 discharge pump	Functional
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AC ELECTRICAL SYSTEM

AC Shore Power	(1) 30A115VAC	Power connection not available, not tested
Shore Power Isolation	Guest 2433 galvanic isolator	Not tested
AC Distribution	AC circuit panel	No access to back side
	Reverse Polarity Indicator	Yes
	Double Pole Main Breaker	Yes
	115VAC GFCI outlets	Circuit or GFCI fault
Battery Charger	Xantrex True Charge 20	Functioal

DC ELECTRICAL SYSTEM

DC charging	Alternator on engine	Amperage measured
(1) West Marine 15020258 Group 31 AGM engine start battery, 12V	Secured in acid proof box	Voltage tested, not load tested
(2) NPD-12-250Ah house batteries in two banks, 12V	Secured in acid proof box	Voltage tested, not load tested
(1) Optima D34M windlass, 12V	Secured in acid proof box	Voltage tested, not load tested
Battery disconnects	Battery switches	

APPLIANCES

Galley stove	Dickinson Adriatic diesel stove/heater	Reported decommissioned, not tested
Cabin heater	Dickinson Antarctic diesel heater	Reported decommissioned, not tested

NAVIGATIONAL EQUIPMENT AND ELECTRONICS

Compass	Ritchie 6", Constellation 6"	Apparently functional
Depthsounder	Garmin Fishfinder 340C	Functional
Radar	Furuno 1621 Mark 2, 10 mile	Functional
GPS	Garmin GPSMAP 545	Functional

Radios	Standard Horizon VHF with Ram3 mic	Functional
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ANCHORING EQUIPMENT

Windlass	Ideal 12VDC horizontal type with twin chain wheels and capstains	Functional, seeping oil at DC cabling
Anchors	Primary: 150# Navy Stockless Secondary: 200# Danforth	Galvanized Galvanized
Rodes	3/8" galvanized chain: 270' & 328' lengths reported	Good condition, not fully inspected,

SAILING EQUIPMENT

Rig Type	Schooner	Winches	(2) Barient 26 Primary
Spars	Spruce, painted		(2) Barient 16 Halyards
Mast step	Both masts keel stepped		(2) Barient 22 (1) Barient 21 foremast
Standing Rigging	SS 1 x 19		(1) Barient 32, (1) Barient 22 Fisherman
Hardware	Inner forestay Highfield lever	Sails	Hank-on jib
Rigging control	Running backstays		Hank-on fore staysail
Chainplates	Bronze plates		Hank-on main staysail
Turnbuckles	Open & closed bodied		Main sail with two reefs
Sheeting	Mainsheet block & tackle		Fisherman in bag
Running rigging	Synthetic		Spare jib, main staysail
Blocks	Fico, HME, wood/bronze	Sail covers	Covers for all sails in use

ADDITIONAL EQUIPMENT

Dinghy	10.5' Pete Culler designed lapstrake wherry with sprit sailing rig, built in 1972	Not tested
Instruments	4" brass barometer & clock	Not tested

SAFETY AND REQUIRED EQUIPMENT

PFDs	Not inventoried	Operator responsible to ensure adequate PFDs per USCG requirements
Throwable PFD	Lifering with ACR SM-2 buoy light	Functional
Lifelines	Synthetic	Functional
Emergency Distress Signal	Flares	Current
Sound signal	Air horn	Functional
Handheld Fire extinguishers	(4) 3A40BC (1) 1A10BC	Inspected April 2024
Automatic Fire Extinguisher	Fireboy MA20600227 rated for 600 cubic feet, with audible alarm and indicator light at helm	Inspected April 2024, alarm proven functional
Placards	Oil discharge	Yes
	Waste discharge	Yes
	CO warning	Not sighted, recommend posting
Waste management plan	Required for vessels over 40'	Not sighted, recommend aquiring
Navigation Rules Book	Required for vessels over 40'	Yes
Carbon Monoxide detectors	ABYC recommended	(3), galley not mounted
Smoke detector	NFPA recommended	(3), galley not mounted
High water bilge alarm	ABYC recommended	Two of three float switches inoperative
Reboarding ladder	Yes	Not swimmer deployable
Navigation lights	Anchor	Portable, not tested
	Side lights	Functional
	Masthead	Functional
	Stern	Functional
Liferaft	30 person	Service 02/2025

VALUATIONS

VALUATION CONSIDERATIONS

- This vessel is an inspected passenger vessel. The work to maintain the vessel to meet the annual COI inspection is considerable and adds value to the vessel. However, this survey does not consider any income from charter work in developing the value of the vessel.
- The undersigned marine surveyor has examined this vessel closely to determine its condition and considered the results of the examination and the Findings and Recommendations below in determining the values shown. See condition statement under General Notes below.

Soldboats.com is a subscription service that provides the Yachtworld listings after a vessel is sold, and shows the reported selling prices. This is some of the best information we have since it provides comparable sales. See comparable listings below:

62 ft 1920 Custom Ted Geary/Blanchard Schooner, Red Jacket
\$95,000
Tacoma, Washington, United States



Contact
Print Listing
Email Listing

Listing Information	
Previous Price:	\$119,000 (November 1, 2017)
Listing Type:	Central/Exclusive
Co-op Type:	Available for co-brokerage
IMT ID:	6169089
Company:	Swiftsure Yachts
Contact:	Swiftsure Yachts
Office Phone:	(206)378-1110
Mobile Phone:	(206)378-1110
Active:	462 Days
Listed Date:	March 22, 2017
Sold Date:	June 27, 2018
Listed Price:	\$95,000
Sold Price:	\$95,000

RED JACKET is also a Geary design of similar size, however it is not an inspected passenger carrying vessel. See description below:

Red Jacket's hull is planked with 2" Port Orford cedar, douglas fir and Alaska yellow cedar over steam bent white oak frames on 12" centers. Keelson and floors are white oak. She is fastened with steel nails. Her hull below the waterline was refastened with galvanized steel screws in 1998. Her seven-ton lead ballast is bolted externally to the hull with monel bolts that were replaced in the mid-1970s. Her deck is Alaska yellow cedar overlayed with plywood and fiberglass. Cabin coachroof is tongue and groove red cedar overlaid with laminated marine plywood and fiberglass. Her cabin sides are Alaska yellow cedar overlaid with teak finished bright.

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BELLINGHAM, WA, USA

56 ft 1934 Motorsailer Pilothouse/William Hand, Guildive
\$115,000
Belfast, Maine, United States

Contact

Print Listing

Email Listing



Listing Information

Previous Price: \$175,000 (December 7, 2020)
Listing Type: Central/Exclusive
Co-op Type: Available for co-brokerage
IMT ID: 6978011
Company: Yacht View Brokerage, LLC - Yacht View Brokerage LLC Annapolis
Contact: John Kaiser
Office Phone: 443-223-7864 Cell anytime
Mobile Phone: 4432237864
Active: 860 Days
Listed Date: February 6, 2019
Sold Date: June 8, 2021
Listed Price: \$150,000
Sold Price: \$115,000
Sale Type: Retail

Guildive, and it's charter business are in turn-key condition. Guildive Cruises, a daysail business founded in 2009, in Castine, Maine, has been offering charters on the Penobscot Bay during the summer season. Upwards of a 1000 guests each summer sail aboard GUILDIVE. The owners are offering the sale of the business if the buyer is interested.

70 ft 1931 Classic Portland Maine Pilot Schooner, TIMBERWIND
\$200,000
Belfast, Maine, United States

Contact

Print Listing

Email Listing



Listing Information

Listing Type: Central/Exclusive
Co-op Type: Available for co-brokerage
IMT ID: 6644987
Company: Artisan Boatworks Inc.
Contact: Alec Brainerd
Office Phone: 2072364231
Mobile Phone: 207-542-0972
Active: 64 Days
Listed Date: March 13, 2018
Sold Date: May 16, 2018
Listed Price: \$240,000
Sold Price: \$200,000

Construction is sawn oak frames with yellow pine and oak planking. She is presently rigged as a schooner, although she has carried a number of different sail configurations in her service history. The bowsprit was added during her conversion to tourist service. Originally powered by large twin diesel engines, these were removed to make room for additional cabin space and her auxiliary power is now provided by a 15 foot yawl boat with a 40 hp Yanmar Diesel. TIMBERWIND is coast guard certified to carry 26 passengers overnight and 46 for daysails.

VALUATION CONSIDERATION RESULTS

This Vessel's Estimated Fair Market Value:	\$140,000
Replacement Value with Like New Vessel:	\$2,000,000

The Fair Market Value is the amount in US dollars a willing, well informed buyer would pay a willing, well informed seller in an open market, neither being compelled to buy or sell, given a reasonable amount of time to sell. It is the value of this vessel as is, where is taking into account the survey findings. It is an opinion of value based upon one or more of the following: actual selling prices of similar vessels from the Marine Consultants, Inc. database, various pricing guides, comparables research and the opinions of other marine industry professionals.

The Replacement Value with Like New is the estimated cost to replace this vessel with a similar, new vessel. In many cases, a particular model is no longer in production and the estimation is based upon similar new vessels currently available. The replacement value was estimated by wooden boat builder Gannon & Benjamin in Vinyard Haven, MA

GENERAL NOTES

- a. This vessel survey does not cover Certificate of Inspection (COI) requirements. The US Coast Guard COI documents and inspections should be separately consulted.
- b. This survey was conducted with the vessel afloat only. Therefore, survey of the hull exterior and underwater gear was not possible. For valuation purposes only, these items are assumed to be in satisfactory condition.
- c. This vessel survey does not include an examination of the sails, furling gear, or the upper portions of the mast and rigging, although the portions of the rig and spars accessible from deck level were observed.

Routine sailboat maintenance should include annual inspection and service of the rig by a qualified rigger. Additionally, chainplates and their fasteners should be periodically examined by disassembly.

The sail inventory was provided by other parties, and sail examination is not included in this survey, although where problems were noted, they are reported.

- d. This vessel survey does not include an intensive engine survey or oil analysis, although the engine was observed externally and run at the dock.
- e. Reference may be made in the findings and recommendations below to USCG (United States Coast Guard) requirements, ABYC (American Boat and Yacht Council) voluntary standards, or NFPA (National Fire Protection Association) voluntary standards.

While the ABYC and NFPA standards are voluntary, they are marine industry consensus standards and are regularly updated by their standards committees to be relevant to current vessels, available

equipment, and safety experience. Compliance is highly recommended for the safety of the vessel and crew.

This vessel was built before the enactment of some of the USCG, NFPA, and ABYC standards in effect today. The vessel is observed with reference to the current standards, and deficiencies thought to be important to the safety of the vessel and personnel are reported. This survey does not and cannot require complete compliance with all of the current voluntary standards.

f. Current advisory and service/maintenance notes include:

- Bilge pumping systems should be periodically tested with water.
- The engine and shaft have exposed rotating machinery. As in any vessel, do not open or enter the engine compartment with the engine operating.
- The current ABYC standards call for an indicator at the helm to warn of a loss of exhaust system cooling water flow.
- The handheld and fixed fire extinguishers were last serviced in April of 2024. Continue to annually inspect and re-tag the extinguishers, and visually inspect monthly per the CFR and NFPA 10.
- Prior to use of the vessel, learn the locations or place on board the USCG or Washington State required safety equipment, including suitable approved lifejackets (PFDs) for each person, the fire extinguishers, approved current signal flares, an approved throwable PFD, and the required discharge of oil, CO warning, and waste placards.

Vessels over forty feet in length are also required to carry a written waste management plan, and a Navigation Rules book (If the vessel will be used on Inland Waters such as the Columbia River).

PRIORITY FINDINGS & RECOMMENDATIONS

1. * The engine exhaust muffler and mixing elbow are corroded and leaking exhaust fumes into the cabin. As a result, the carbon monoxide/smoke detector has been removed from the galley.
Recommendation: Repair or replace the muffler and mixing elbow. Reinstall the carbon monoxide/smoke detector in the galley and prove operational.



2. * When the 115VAC outlets breaker was switched on, the GFCI outlet on the aft gally bulkhead buzzed, and an outlet tester indicated hot/ground reversed. Also, the outlet breaker is unlabelled.
Recommendation: Replace the GFCI outlet and prove the circuit correctly wired and all GFCI outlets functional. Label the breaker.

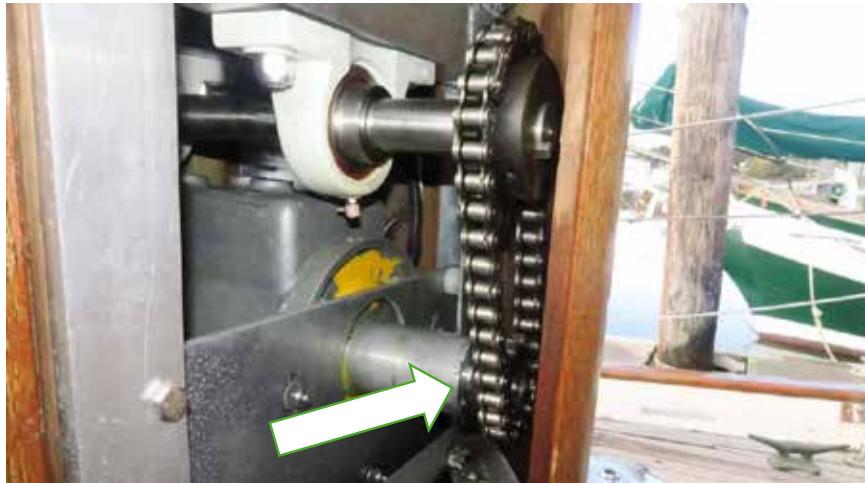
*** The Resolution of the above asterisk items is considered essential for the continued safe use or operation of this vessel.**

OTHER FINDINGS AND RECOMMENDATIONS

3. The starboard forward cable steering sheave is broken, and the port mid sheave is frozen.
Recommendation: Replace the broken sheave, and free up and lubricate the frozen sheave.



4. The hydraulic steering pump leaks oil.
Recommendation: Service to eliminate leaking.



5. The engine driven bilge pump piping has a broken pipe nipple, causing the pump to suck air and not pump water. Also, the pump discharge does not include a vented loop to prevent siphoning.
Recommendation: Replace the broken pipe nipple, and add a vented loop to the discharge plumbing.



6. When the midship (salon bilge) high water alarm float switch was raised the alarm sounded, but when the forward and aft float switches were raised, the alarm did not sound. A high water alarm system is an ABYC standard.
Recommendation: Service the high water alarm system as needed for reliable operation.
7. The midship (salon bilge) bilge pump float switch interferes with the discharge hose, preventing lifting and pumping.
Recommendation: Relocate the float switch for clearance.
8. This vessel is fitted with a PSS shaft seal which utilizes a stainless steel ring secured to the propeller shaft with set screws. This ring must not move on the shaft, or a leak will develop.
Recommendation: It is recommended that a stainless steel hose clamp, or retaining ring be installed on the shaft against the face of the stainless rings, to prevent the chance of the ring sliding forward if the set screws do not hold or should loosen.

Also note: PYI, the manufacturer of this seal, recommends that the bellows hose be replaced every six years.



9. The diesel stove Racor fuel filter bowl is not fitted with a metal heat shield.
Recommendation: Replace the bowl with a metal bowl to meet the ABYC standard.



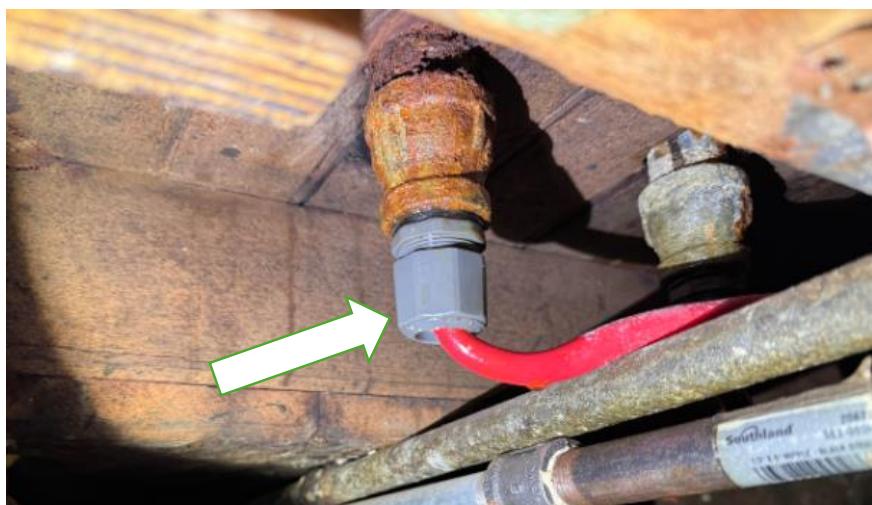
10. Some of the fuel line fittings are corroded at the aft tanks.
Recommendation: Replace the fuel lines with corroded fittings.



11. The alternator positive output cable has been led off of the engine and it is over 40 inches in length.
Recommendation: This cable should be fused at the alternator per ABYC standard 11.10.1.1.1, and at the battery or point of connection to the DC electrical system unless it is a battery isolator. If the ampacity of the conductor is equal to or greater than the output of the alternator, overcurrent protection is not required at the alternator.

12. The holding tank monitor is inoperative.
Recommendation: Repair as needed.

13. There is oil leaking from the windlass positive cable connection.
Recommendation: Plan to repair windlass oil leak.



14. The head sink drain hose is age cracked.

Recommendation: Replace this hose with suitable reinforced marine hose.

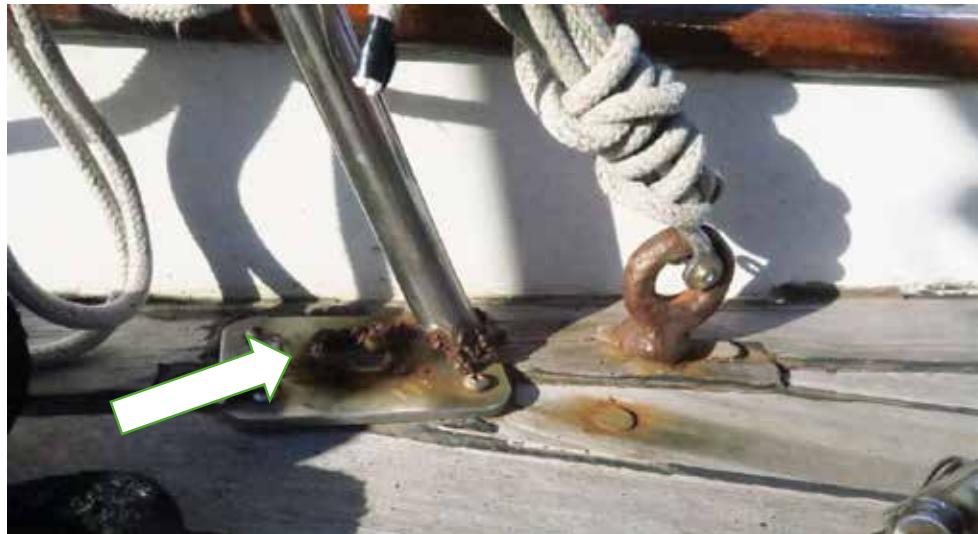
15. There is unsecured lead ballast in the hull aft of the engine.

Recommendation: Secure this lead ballast.



16. There is a broken weld on a pushpit mounting pad.

Recommendation: Reweld mounting pad.



17. The reboarding ladder is stowed in the vessel interior. The ABYC standard calls for a suitable re-boarding means, accessible and deployable by a swimmer in the water.

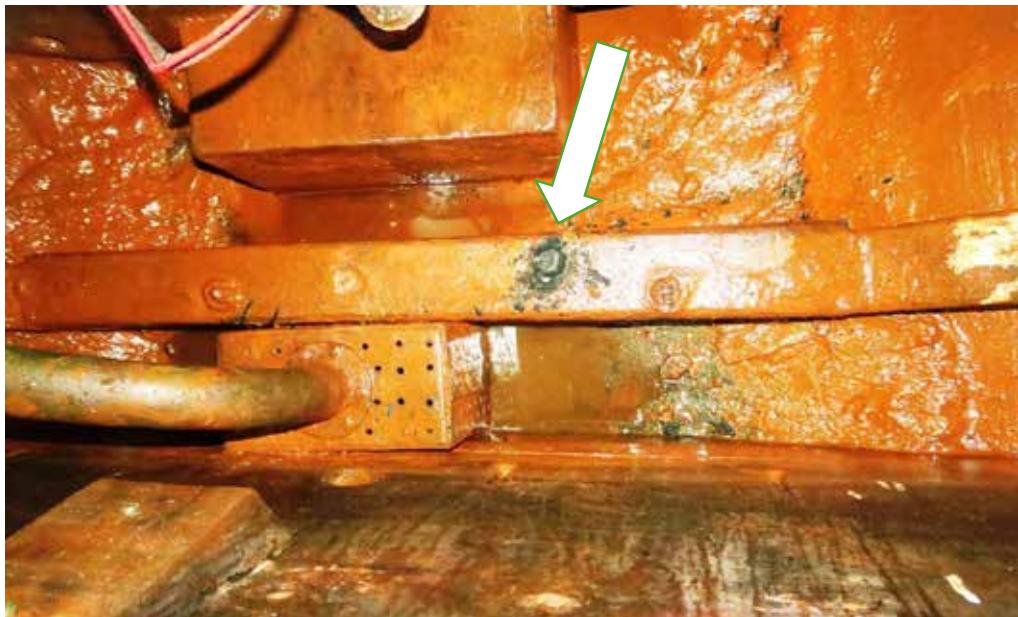
Recommendation: Install a permanently accessible swim ladder.

18. Garbage and Oil discharge placards were sighted, but a CO warning sticker was not.

Recommendation: Install a CO warning sticker.

19. Some of the original bolts securing the floor timbers to the frames are corroded in the galley bilge (see sample photo below, with arrow indicating corroded bolt), but appeared to be in better condition further forward.

Recommendation: Plan to re-fasten the floor timbers to the frames as needed.



20. Some soft wood was noted in a floor timber supporting the main mast step.

Recommendation: Plan to replace the soft wood and restore the floor timber to original integrity.



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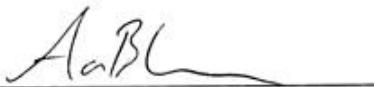
BELLINGHAM, WA, USA

This examination has been conducted without making removals or opening up to expose areas or components ordinarily concealed, or test boring, testing for tightness, pressure testing tanks, trying out machinery, or performing an exhaustive survey of the vessel's AC or DC electrical system or bonding system, and does not, therefore, address any damages and/or deficiencies which might have been revealed if such procedures had been executed. No evaluations were made and no opinions are offered relative to the vessel's engineering, performance, or stability, or of the sizing or suitability of any installed components, design, or feature.

This report is only a statement of opinion and is neither a guarantee nor a warranty relative to: the merchantability of the vessel, the valuation of the vessel, the condition of the vessel or its machinery or equipment, or any unforeseen or undetected damages or other conditions that may exist.

This limited report is issued in accordance with the Terms and Conditions below, and/or the Work Order of this survey, which Terms and Conditions apply to the attending marine surveyor and Marine Consultants, Inc. Acceptance of this report or its use for any purpose serves as acknowledgement and agreement with these terms and conditions.

The undersigned certifies that the statements in this report are true and correct; that the analyses, opinions, and conclusions are limited only by the reported assumptions and limiting conditions, and are my personal, impartial, and unbiased professional analyses, opinions, and conclusions. I have no bias with respect to the property that is the subject of this report or to the parties involved. My engagement in this assignment was not contingent upon developing or reporting pre-determined results. My compensation for this assignment is not contingent upon the development or reporting of a predetermined value or direction in value that favors the cause of the client.



Marine Consultants
Aaron Bandstra NAMS-CMS



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BELLINGHAM, WA, USA

TERMS AND CONDITIONS

Marine Consultants 2023 LLC, doing business as Marine Consultants (hereafter referred to as MC) agrees to undertake the work requested by Customer only on the following terms and conditions which shall apply to all work done by MC and all reports relating to such work.

1. Additional Services: Any additional work requested or authorized by Customer, either verbally or in writing, shall be subject to these terms and conditions. Additional work performed will be charged at MC's normal hourly rate.

2. Expenses: Customer shall pay MC all costs, including but not limited to travel expenses, hotel, meals, lodging, telephone, and other expenses relating to the work requested.

3. Payment and Interest: Payment of all fees and expenses shall be due upon completion of the work unless other arrangements are made. All payments, if unpaid when due, shall bear interest at the rate of 1.5% per month from the date due until paid.

4. Lien: Customer grants MC a lien (including a maritime lien) on the vessel and its equipment involved in this work order until all fees and expenses have been fully paid. MC shall have and retain all other legal rights it may have, until the fees and expenses have been paid in full. Customer, including the vessel, and/or the person directly contracting MC for this work remain jointly and severally responsible for the charges until fully paid. The person signing this work order warrants that he has authority to bind all such parties to these fees and expenses.

5. Limited Report: Customer is cautioned that this is a limited report representing a limited inspection by visual means and soundings. Inspection of areas normally concealed, areas requiring disassembly of the vessel, scaling of masts, or the operation of equipment is specifically not included unless otherwise agreed upon in writing. Such reports constitute only statements of opinion and are not to be construed or considered as representations, warranties, or guarantees. MC disclaims any expertise regarding insurance. Any statements made by surveyor are not to be construed in any way as bearing upon the possible existence of insurance coverage. Except as provided herein, MC makes no warranties, express or implied, regarding the work performed, including, without limitation, any implied warranties of merchantability or fitness for a particular purpose. The representations and warranties made by MC in this agreement apply only to the work performed by MC and will be invalid if the Customer is in breach of these terms and conditions.

6. Limited Liability: MC shall not be liable to Customer for any claim, loss, cost, penalty, or damages of whatsoever kind or nature arising out of, in connection with, or incident to, the work requested, except that caused by the direct sole negligence of MC. Negligence shall not be legally presumed but must be affirmatively established. MC shall not be liable in any event for any loss, cost, penalty, or damages in excess of the total cost paid by Customer for the work. MC shall not be liable to Customer except on the limited basis identified above. MC shall specifically not be liable for incidental, special or consequential damages, nor loss of use, loss of profits/earnings, crew wages, shares, salvage, repair, tug expense, demurrage, loss of time, loss of freight, loss of charter and/or similar and/or substituted expenses.

In addition, MC shall not be liable to Customer on any legal basis other than negligence as stated above. Liability to Customer for breach of contract, breach of warranty of workmanlike service, strict and/or products liability, liability for breach of warranties of merchantability or fitness for a particular purpose or any other legal theory or basis for liability, and liability (directly or indirectly) to Customer's insurers, are specifically agreed by Customer and MC to be excluded. It is understood by Customer that MC's charges for services are based upon this limited liability. MC agrees to assume additional liabilities only if requested by Customer and a written agreement setting forth liabilities and additional charges are signed by both MC and Customer.

7. USCG: The USCG and other government agencies may require additional equipment and/or modifications to the vessel depending upon its use. MC shall not be liable for anticipation of these requirements.

8. Markings: MC assumes no legal or financial liability for any cosmetic work necessary to remove marks or blemishes caused by the inspection.

9. Notice, Claim, Time Limits or Suit: MC shall in no event be liable even on the limited basis identified above, unless notice of claim thereof is presented in writing to MC within ninety (90) days of completion of the work. Customer must also commence suit on any claim or controversy arising under this work order or the work performed pursuant to the work order, within six (6) months from completion of work. If Customer fails to do either then MC is discharged from all liability to Customer on any basis.

10. Law and Venue: Any work performed by MC and any report issued by MC shall be construed in accordance with the laws of the State of Washington. Any action, claim, or suit between the parties must be brought in the state courts located at Bellingham, Whatcom County, Washington. The prevailing party in any litigation shall be entitled to recover all costs including reasonable attorneys' fees.

11. Specifications: MC shall not be liable for the accuracy of dimensions, capacities, ratings, equipment, inventory, etc. This data is often obtained from outside sources and is included for general descriptive purposes only.

12. General: These terms (together with any scope of work): (i) represents the entire understanding of the parties with respect to the subject matter covered; (ii) supersedes all prior and contemporaneous oral understandings with respect to such subject matter; (iii) may only be amended in a writing signed by both parties; (iv) binds and inures to the benefit of the parties and their respective successors, permitted assigns, agents and representatives; and (v) constitutes material consideration for the agreement between MC and Customer without which MC would not agree to perform the work. The failure of either party at any time to require performance of any provision of these terms shall not limit such party's right to enforce such provision, nor shall any waiver of any breach of any provision of these terms constitute a waiver of any succeeding breach of such provision or a waiver of such provision itself. Any provision of these terms which shall prove to be invalid, void, or illegal shall in no way affect, impair, or invalidate any other provision of these terms, and the remaining provisions shall nevertheless remain in full force and effect.

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Port Bow



Port Stern



Ideal Windlass and bow



Decks and cabin looking forward



Decks and cabin looking aft



Decks and cabin looking aft

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Lapstrake Dinghy



Pilothouse



Aft wheel and pilothouse



Forepeak with anchor locker bulkhead, and foward fuel tanks



Forward cabin



Salon looking forward



Salon looking aft



Head compartment



Galley looking aft



Galley looking forward



Galley looking to port



Pilothouse helm



Helm instruments



Detroit Diesel engine



Engine tag



Documentation number